



# ROGERS WALK/BIKE ACTION PLAN

## OVERVIEW

Rogers is the third largest city in NWA with a population of 58,895. Urban to suburban development is found throughout its large land area that covers 38.1 square miles. Rogers’ developing trail network consists of 28 miles of shared use paved trails and sidepaths and is quickly expanding. This includes the Razorback Regional Greenway linking the west side of Rogers to Bentonville and western Lowell. Other trails throughout the city are expanding to connect local schools, parks and other destinations. New developments in and around Lake Atalanta and the downtown area include several types of walking and biking trails including natural surface trails (hiking and mountain biking). However, several major roadways bisect Rogers including I-49, US 71B, US 62, and New Hope Road making connections across town more challenging. Continuing to link neighborhoods to local and regional destinations such as Lake Atalanta, the Pinnacle Hills shopping area, and downtown, in addition to surrounding communities will serve as key next steps for the City of Rogers.

### Regional Destinations

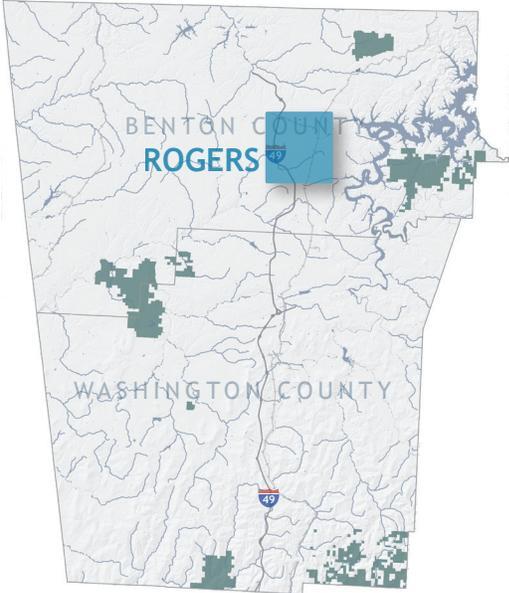
- » Razorback Regional Greenway
- » Lake Atalanta
- » Pinnacle Hills Shopping Area
- » Northwest Arkansas Community College
- » Downtown

### Other Key Destinations

- » Local parks and schools, public library
- » Aquatics Center, Adult Wellness Center, Activity Center
- » Residential areas

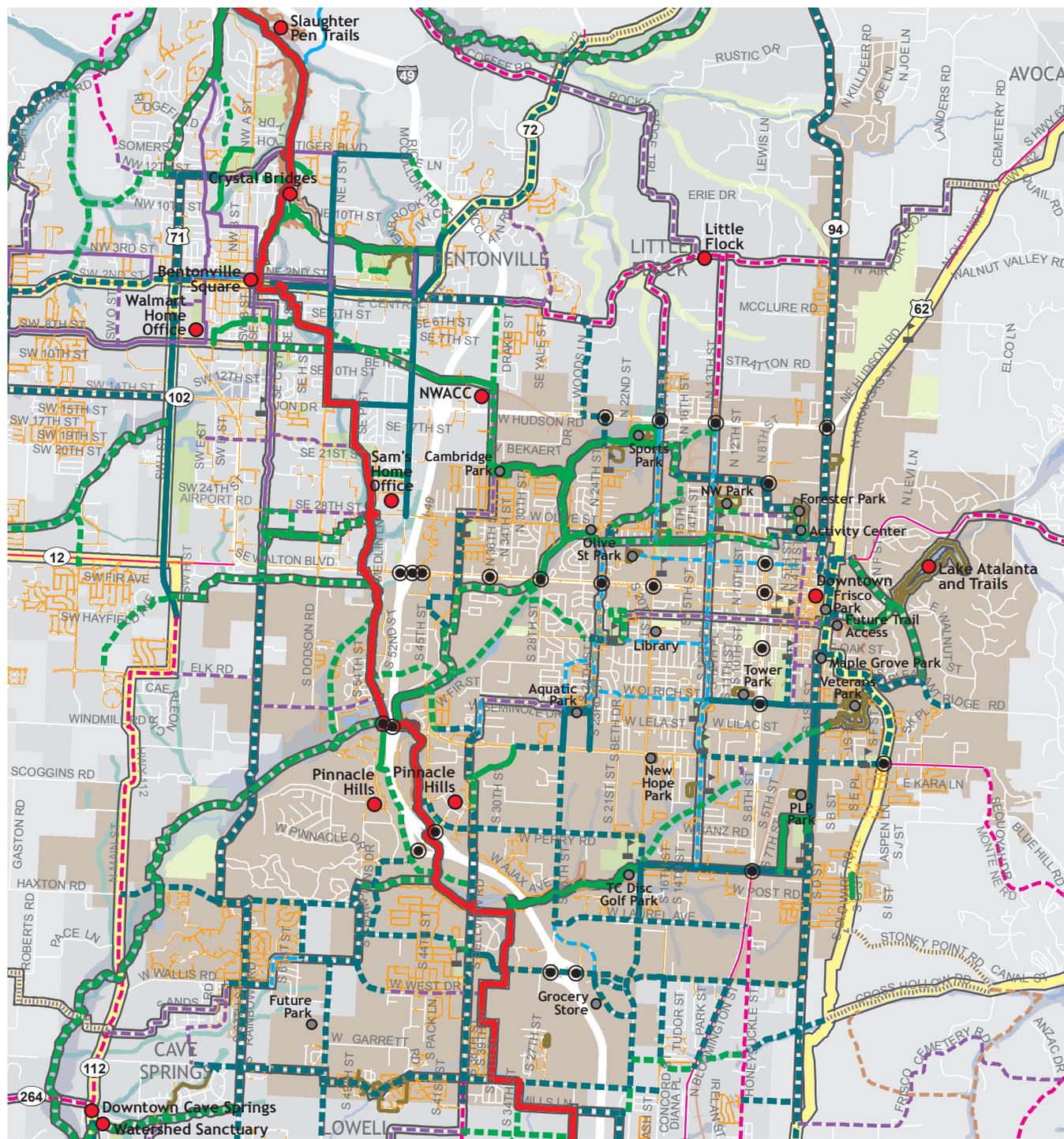


### REGIONAL LOCATION MAP

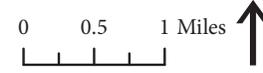


*Clockwise from upper left: Rogers High School/Razorback Regional Greenway undercrossing of I-49; Lake Atalanta; Bicycles in front of Old Wire Elementary School*

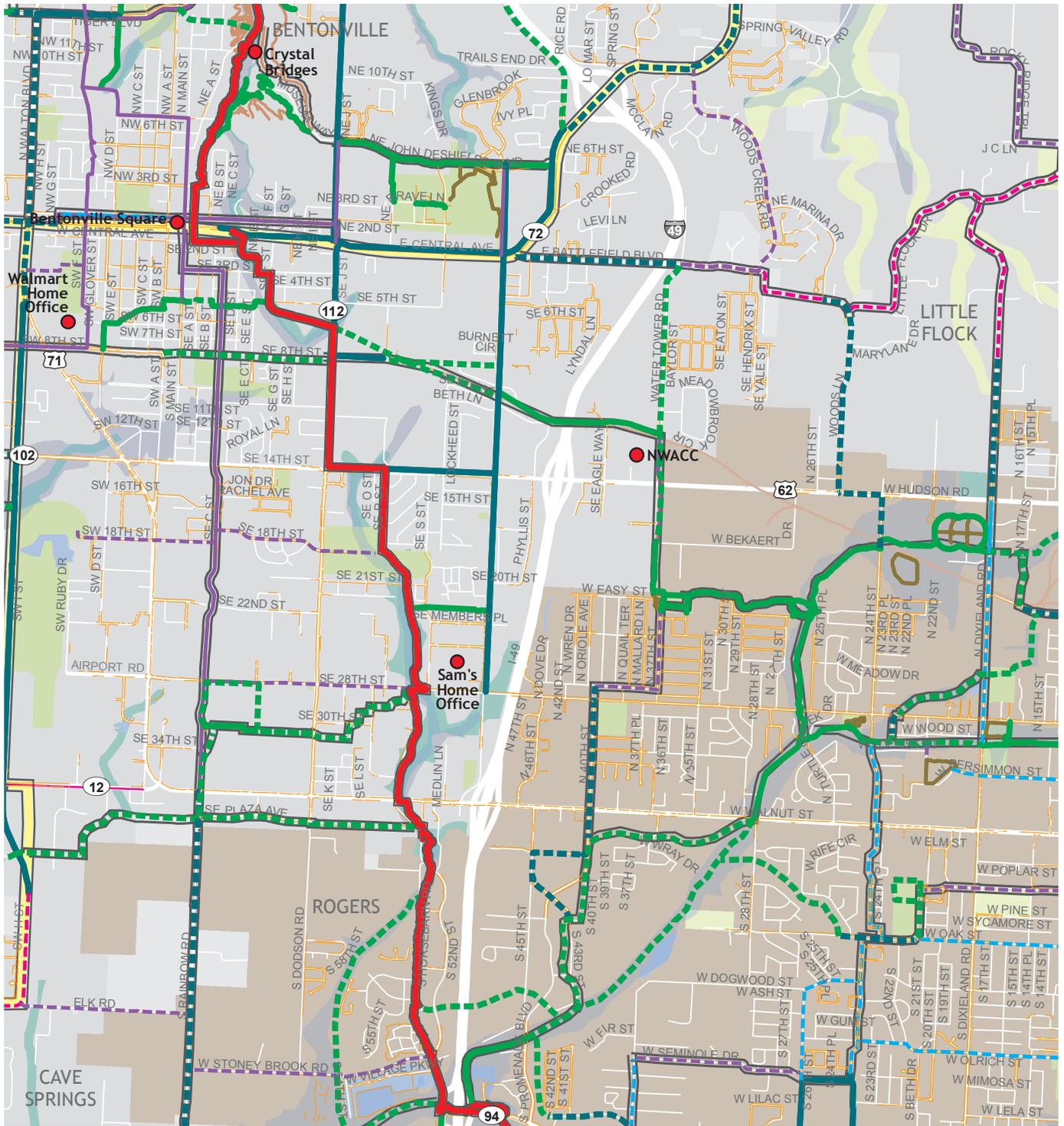
# MAP 6.21 ROGERS COMMUNITY PLAN



<ul style="list-style-type: none"> <li> Razorback Greenway</li> <li> Regional Network</li> <li> Heritage Trail</li> <li> Regional Destination</li> <li> Local Destination</li> <li> School</li> </ul>	<p><b>Shared Use Facilities</b></p> <ul style="list-style-type: none"> <li> Proposed Shared Use Paved Trail</li> <li> Existing Shared Use Paved Trail</li> <li> Proposed Sidewalk</li> <li> Existing Sidewalk</li> <li> Proposed Natural Surface Trail</li> <li> Existing Natural Surface Trail</li> <li> Crossing Improvement</li> </ul>	<p><b>Bicycle Facilities</b></p> <ul style="list-style-type: none"> <li> Proposed Separated Bikeway</li> <li> Existing Separated Bikeway</li> <li> Existing Shared Roadway</li> <li> Proposed Shared Roadway</li> <li> Gravel Grinding Route</li> </ul>	<ul style="list-style-type: none"> <li> Proposed Shoulder Improvement</li> <li> Existing Shoulder</li> </ul> <p><b>Pedestrian Facilities</b></p> <ul style="list-style-type: none"> <li> Proposed Sidewalk</li> <li> Existing Sidewalk</li> <li> Neighborhood/Park Trails</li> </ul>
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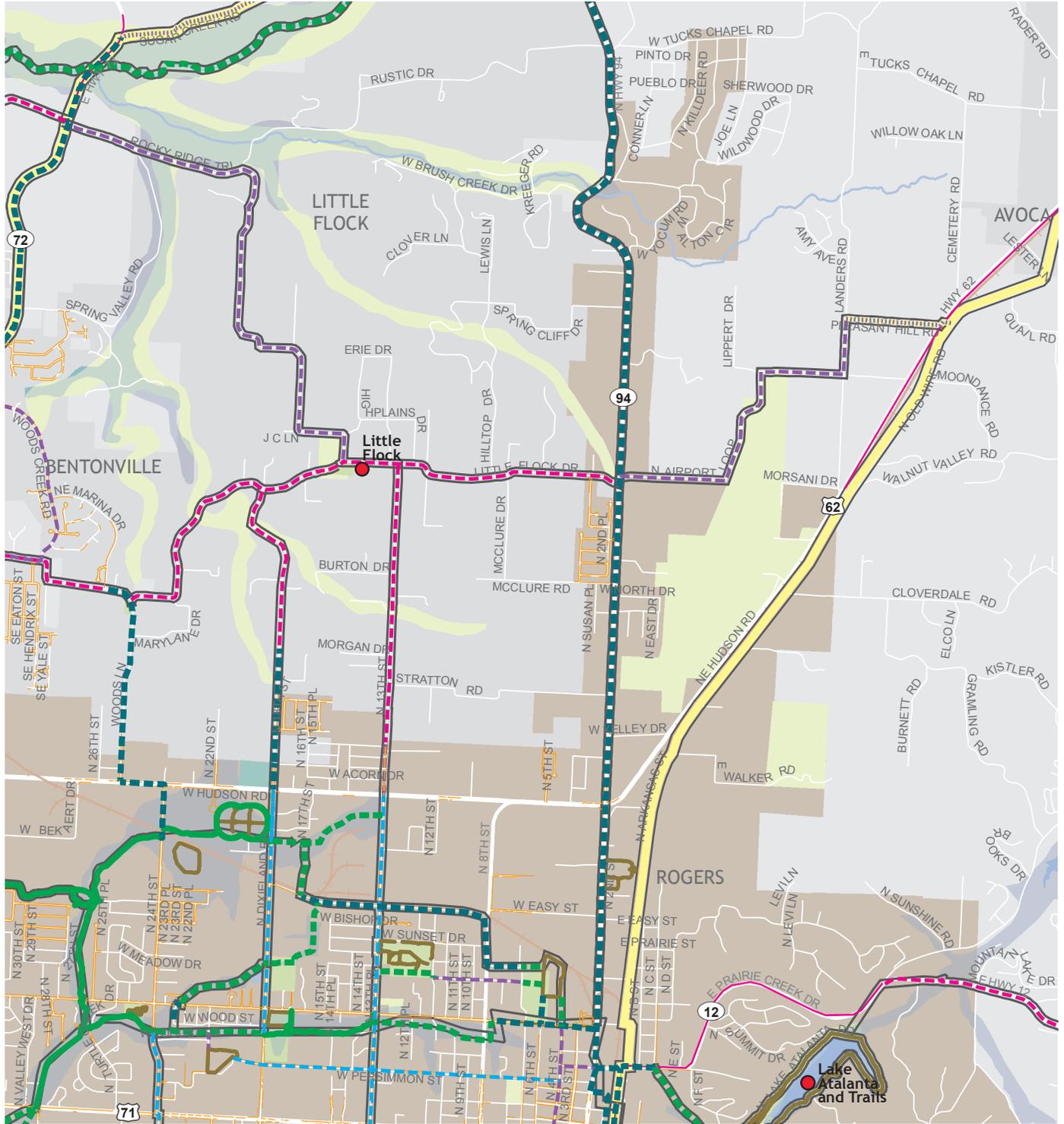
# MAP 6.21-NW ROGERS NORTHWEST QUADRANT



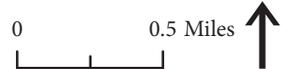
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| <ul style="list-style-type: none"> <li> Razorback Greenway</li> <li> Regional Network</li> <li> Heritage Trail</li> <li> Regional Destination</li> <li> Local Destination</li> <li> School</li> </ul> | <p><b>Shared Use Facilities</b></p> <ul style="list-style-type: none"> <li> Proposed Shared Use Paved Trail</li> <li> Existing Shared Use Paved Trail</li> <li> Proposed Sidewalk</li> <li> Existing Sidewalk</li> <li> Proposed Natural Surface Trail</li> <li> Existing Natural Surface Trail</li> <li> Crossing Improvement</li> </ul> | <p><b>Bicycle Facilities</b></p> <ul style="list-style-type: none"> <li> Proposed Separated Bikeway</li> <li> Existing Separated Bikeway</li> <li> Existing Shared Roadway</li> <li> Proposed Shared Roadway</li> <li> Gravel Grinding Route</li> </ul> | <ul style="list-style-type: none"> <li> Proposed Shoulder Improvement</li> <li> Existing Shoulder</li> </ul> <p><b>Pedestrian Facilities</b></p> <ul style="list-style-type: none"> <li> Proposed Sidewalk</li> <li> Existing Sidewalk</li> <li> Neighborhood/Park Trails</li> </ul> |
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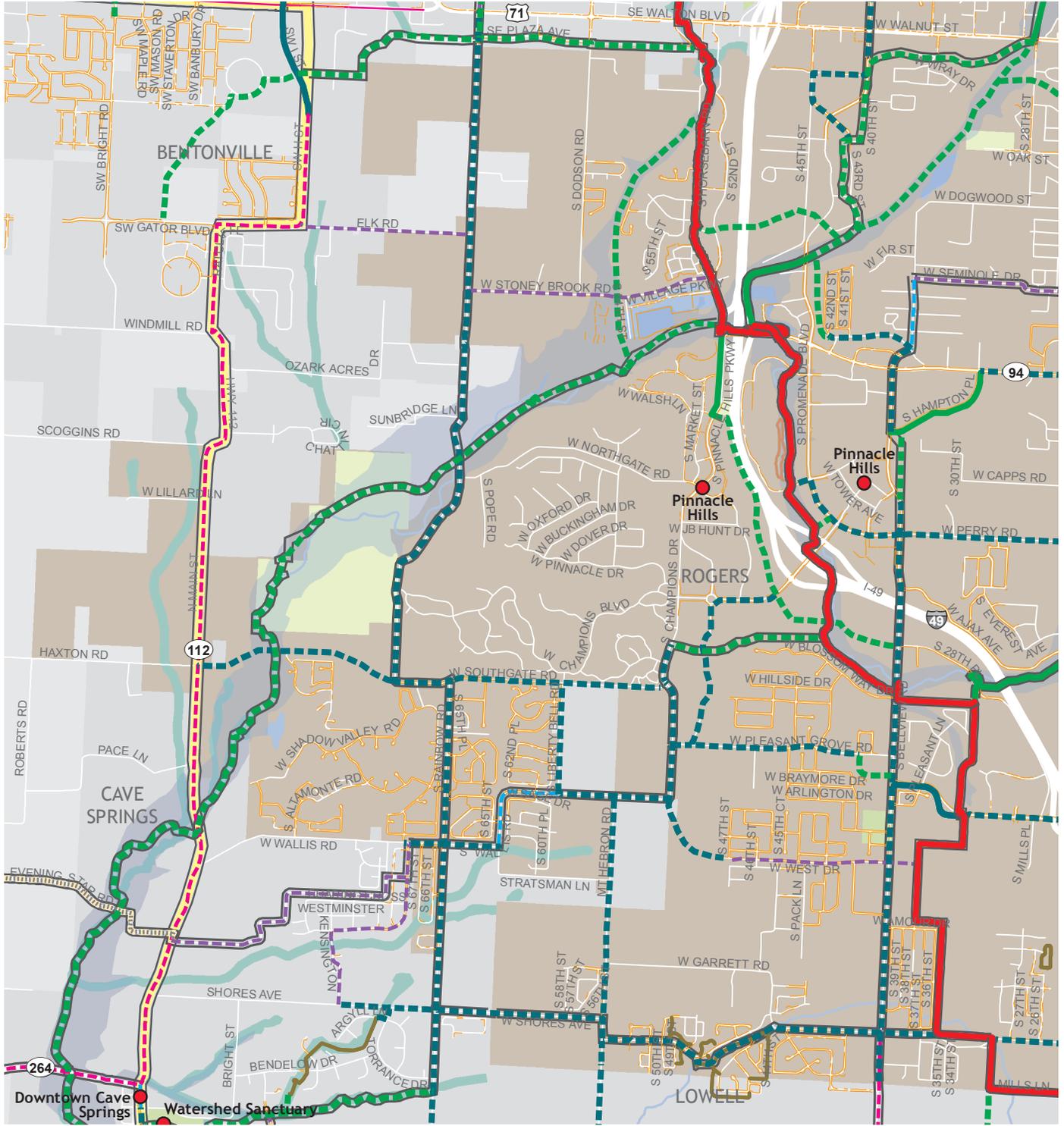
# MAP 6.21-NE ROGERS NORTHEAST QUADRANT



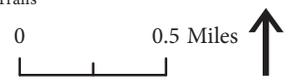
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|---|---|---|--|
| <ul style="list-style-type: none"> <li> Razorback Greenway</li> <li> Regional Network</li> <li> Heritage Trail</li> <li> Regional Destination</li> <li> Local Destination</li> <li> School</li> </ul> | <p><b>Shared Use Facilities</b></p> <ul style="list-style-type: none"> <li> Proposed Shared Use Paved Trail</li> <li> Existing Shared Use Paved Trail</li> <li> Proposed Sidepath</li> <li> Existing Sidepath</li> <li> Proposed Natural Surface Trail</li> <li> Existing Natural Surface Trail</li> <li> Crossing Improvement</li> </ul> | <p><b>Bicycle Facilities</b></p> <ul style="list-style-type: none"> <li> Proposed Separated Bikeway</li> <li> Existing Separated Bikeway</li> <li> Existing Shared Roadway</li> <li> Proposed Shared Roadway</li> <li> Gravel Grinding Route</li> </ul> | <ul style="list-style-type: none"> <li> Proposed Shoulder Improvement</li> <li> Existing Shoulder</li> </ul> <p><b>Pedestrian Facilities</b></p> <ul style="list-style-type: none"> <li> Proposed Sidewalk</li> <li> Existing Sidewalk</li> <li> Neighborhood/Park Trails</li> </ul> |
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# MAP 6.21-SW ROGERS SOUTHWEST QUADRANT



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|---|---|---|---|
| <ul style="list-style-type: none"> <li> Razorback Greenway</li> <li> Regional Network</li> <li> Heritage Trail</li> <li> Regional Destination</li> <li> Local Destination</li> <li> School</li> </ul> | <p><b>Shared Use Facilities</b></p> <ul style="list-style-type: none"> <li> Proposed Shared Use Paved Trail</li> <li> Existing Shared Use Paved Trail</li> <li> Proposed Sidepath</li> <li> Existing Sidepath</li> <li> Proposed Natural Surface Trail</li> <li> Existing Natural Surface Trail</li> <li> Crossing Improvement</li> </ul> | <p><b>Bicycle Facilities</b></p> <ul style="list-style-type: none"> <li> Proposed Separated Bikeway</li> <li> Existing Separated Bikeway</li> <li> Existing Shared Roadway</li> <li> Proposed Shared Roadway</li> <li> Gravel Grinding Route</li> </ul> | <p><b>Pedestrian Facilities</b></p> <ul style="list-style-type: none"> <li> Proposed Shoulder Improvement</li> <li> Existing Shoulder</li> <li> Proposed Sidewalk</li> <li> Existing Sidewalk</li> <li> Neighborhood/Park Trails</li> </ul> |
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## Key Needs & Recommendations for Rogers

Topic	Key Issues & Notes
<i>Regional Needs</i>	<ul style="list-style-type: none"> <li>» Continue developing connections to the Razorback Regional Greenway.</li> <li>» Continue trail development connecting Lake Atalanta and the downtown area.</li> <li>» I-49, US 62, and US 71B limit connectivity to surrounding communities.</li> <li>» Explore opportunities to connect to Beaver Lake and Hobbs State Park.</li> </ul>
<i>Other Key Needs</i>	<ul style="list-style-type: none"> <li>» 2014-2015 Rogers Greenways &amp; Trails Map serves as a key guide for the bicycle and pedestrian network.</li> <li>» North/south rail line limits connectivity to the east side of Rogers.</li> <li>» Several high speed, high traffic volume corridors limit circulation through the city (New Hope Road, Walnut Street, 8th Street, I-49, Olive Street, Hudson Road, and Dixieland Road).</li> <li>» Extensive pedestrian network exists – limited bicycle facilities are present.</li> </ul>
Facility Recommendation	Recommendation Detail
<i>Sidewalks</i>	<ul style="list-style-type: none"> <li>» Continue developing sidewalks with new development, and continue filling sidewalk gaps as necessary. Continue integrating shared use paved trails and sidepaths as part of pedestrian network improvements.</li> </ul>
<i>Intersections</i>	<ul style="list-style-type: none"> <li>» Crossing facilities are well constructed in key locations such as the downtown area and other new facilities through town (such as Perry Road and 26th Street). Continue implementing crossing improvements, especially where feasible along higher speed/higher traffic volume roadways such as Walnut Street, 8th Street, Hudson Street, Olive Street, New Hope Road, and Dixieland Road. Innovative intersection treatments (see design guidelines in Appendix A) should be implemented along the developing bicycle and pedestrian network.</li> </ul>
<i>On-Street Bike Facilities</i>	<ul style="list-style-type: none"> <li>» Separated Bikeway – 13th Street &amp; 24th Street: Identified on the 2014-2015 Rogers Greenways &amp; Trails Map as a bike route, these streets are key north/south routes through the city that have appropriate width in many places for incorporating separated bicycle facilities. These streets link several local schools and destinations.</li> <li>» Separated Bikeway – Olrich Street, Persimmon Street, and Oak Street: Similarly, these streets (Olrich and Oak Street identified in the 2014-2015 Rogers Greenways &amp; Trails Map as a bike route) are key routes (east/west) through the city that have appropriate width in many places for incorporating separated bicycle facilities. These streets link several local schools and destinations.</li> <li>» Shared Roadways – 4th Street and Cypress Street: 4th Street (north/south) and Cypress Street (east/west) offer lower traffic alternatives in connecting to the downtown area, requiring little improvements in physical infrastructure.</li> </ul>



### Key Needs & Recommendations for Rogers (continued)

Facility Recommendation	Recommendation Detail
<i>Shared Use Facilities</i>	<ul style="list-style-type: none"> <li>» Shared Use Paved Trails – Northern Loop: Continue developing shared use paved trail links from the Promenade Trail to the Turtle Creek Trails and in connecting the Sports Park through downtown to Cherry Street and the developing trails between downtown and Lake Atalanta. These trails serve as key components of the northern loop trail through the city.</li> <li>» Shared Use Paved Trails – Southern Loop: Continue developing links along riparian corridor between Rogers High School and Veterans Park, connecting southern Rogers toward the downtown area and Lake Atalanta.</li> <li>» Shared Use Paved Trails/Sidepaths – Razorback Regional Greenway links: Continue developing branches to/from the Razorback Regional Greenway, especially continuing southwest along Osage Creek toward Cave Springs and the Watershed Sanctuary. A sidepath link along Promenade Boulevard will serve as a key link to the Pinnacle Hills shopping area and the Razorback Regional Greenway. Continue developing linkages to/from the Razorback Regional Greenway that also serve as connections to Bentonville.</li> <li>» Sidepaths – 1st Street, Arkansas Street, and Monte Ne Road: Continue sidepath network development providing key connection between the downtown area, links to Lake Atalanta, and recent trail developments in southeastern Rogers bridging the gap between several schools, parks, and neighborhoods.</li> <li>» Sidepaths – AR 94 and Old Wire Road: To the north along AR 94, this connection provides a pathway from downtown through the northern city limits of Rogers, Little Flock, and to Pea Ridge. To the south along Old Wire Road, this link provides a pathway to Lowell.</li> <li>» Natural Surface Trails – Lake Atalanta: Continue developing natural surface trail network and access trails in and around Lake Atlanta. Key connection locations include Cherry Street, Poplar Street, Pine Street, and Oak Street in addition to other shared use paved trail connections.</li> </ul>
Other Topics	Notes
<i>Multi-Modal Connections</i>	<ul style="list-style-type: none"> <li>» Rogers is connected to Ozark Regional Transit routes 51 &amp; 52 with 12 stops throughout Rogers. Bicycle parking, infrastructure connectivity, and other amenities should be considered here. Please see <a href="http://www.ozark.org">www.ozark.org</a> for route details.</li> </ul>
<i>Heritage Trail</i>	<ul style="list-style-type: none"> <li>» The NWA Heritage Trail connects north/south through Rogers along Old Wire Road.</li> </ul>

### Program & Policy Recommendations

The table on the following page presents a summary of recommendations organized by category which identifies the recommended program, time-frame for implementation, and lead entity. The table identifies Bronze (short-term), Silver (mid-term) and Gold (long-term) actions that can help achieve recognition as a Walk and Bicycle Friendly Community.

For many of these actions, there is an opportunity for the communities of the region to share resources, with individual communities participating in regional programs, attending trainings or meetings convened at the regional scale, or implementing regionally developed programs at the local level. Recommendations specific to the Six E's (Engineering, Education, Encouragement, Enforcement, and Evaluation (with Equity considered broadly through all)) are found below. Economy is included as an additional category to help demonstrate the benefits of implementing all of the E's. Refer to Appendix D for detailed guidance on implementing each item, including a description of recommended actions, regional and local roles, as well as sample programs.

## Program &amp; Policy Recommendations

Program	Term	Lead Entity
<b>Engineering</b>		
Complete Streets Policy	Medium	City of Rogers
ADA Transition Plans	Medium	City of Rogers
Non-Motorized Transportation Training for Engineers and Planners	Short	NWARPC, City of Rogers
Bicycle Parking	Medium	NWARPC, Northwest Arkansas Council, City of Rogers
Conform to APBP guidelines, increase amount/quality at local/regional destinations, access at multi-family dwellings and public housing, bike parking requirements for new development		
Enhanced Funding for Bike and Pedestrian Projects	Medium	NWARPC, City of Rogers
Develop and implement streetscape design guidelines that foster a pleasant and comfortable environment for pedestrians and bicyclists.	Medium	City of Rogers
Create program and dedicate funding to in-fill sidewalk projects.	Medium	City of Rogers
Bike/Pedestrian Facility Inclusion in Engineering Documents, Plans, and Drawings	Short	NWARPC, City of Rogers
Transportation Planning and Land Use Planning Considerations	Long	NWARPC, City of Rogers
Encourage mixed use and higher density development		
<b>Education</b>		
Safe Routes to School	Short	City of Rogers
Education campaign including motorists, walkers/runners, and bicyclists	Short	Northwest Arkansas Council, City of Rogers
School classroom programming	Medium	City of Rogers
<b>Encouragement</b>		
Razorback Regional Greenway Transportation Promotion	Short	NWARPC, Northwest Arkansas Council, City of Rogers
Equity Oriented Programs	Short	NWARPC, Northwest Arkansas Council, City of Rogers
Open Streets Events	Medium	Northwest Arkansas Council, City of Rogers
Bicycle Friendly Business Program	Medium	City of Rogers
Form bicycle and pedestrian related advocacy group(s)/friends group(s)	Medium	City of Rogers, Bike Bentonville, BCO, IMBA
Expand and improve bike share program	Medium	City of Rogers
<b>Enforcement</b>		
Annual Meeting with Police, Planners and Engineers to Evaluate Collision Trends, Infrastructure Needs and Areas for Targeted Enforcement	Medium	City of Rogers
Bicyclist and Motorist Ticket Diversion Program	Long	City of Rogers Law Enforcement
<b>Evaluation</b>		
Work with AHTD to execute bicycle & pedestrian planning on state roadways through Rogers	Short	City of Rogers
Walking, Bicycling and Trails Report Card	Medium	City of Rogers
<b>Economy</b>		
Economic Impact Report for Razorback Regional Greenway and Trails	Medium	Northwest Arkansas Council, City of Rogers
Bicycle and Walking Tourism Strategy	Medium	Northwest Arkansas Council, City of Rogers

## Design Guideline Examples For Rogers

Below are some highlights from this plan's bicycle and pedestrian design guidelines that are relevant to Rogers' recommendations. See the full Appendix A for more information and other design guide resources.

# BICYCLE BOULEVARD

**Signs and Pavement Markings** identify the street as a bicycle priority route.



**Enhanced Crossings** use signals, beacons, and road geometry to increase safety at major intersections.

**Partial Closures** and other volume management tools limit the number of cars traveling on the bicycle boulevard.

**Speed Humps** manage driver speed.

**Curb Extensions** shorten pedestrian crossing distance.

**Mini Traffic Circles** slow drivers in advance of intersections.



# BUFFERED BIKE LANE

Parking side buffer designed to discourage riding in the "door zone"

Color may be used at the beginning of each block to discourage motorists from entering the buffered lane

MUTCD R3-17 (optional)

